

Anchor Springs, Littlehampton

OUTLINE CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN

for Proposed Hotel
on behalf of Premier Inn Hotels
2024/8391/CEMP01

April 2025

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1 INTRODUCTION

1.1 Report Context

- 1.1.1 RGP is commissioned by Premier Inn Hotels to provide transport and highways input in support of the proposed hotel at Anchor Springs, Littlehampton, BN17 6AT ("the site").
- 1.1.2 The site currently comprises a former retail food store with accompanying car park offering circa 100 spaces. The development proposals comprise of the redevelopment of the site to provide a 130-bedroom hotel to be operated by Whitbread under the Premier Inn brand, with the hardstanding car parking area retained.
- 1.1.3 As illustrated in the proposed Site Plan attached hereto at **Appendix A**, vehicle access would be afforded under the existing arrangements via Avon Road, including the formation of an inset delivery bay to the rear of the hotel. The main guest and visitor entrance to the hotel would be via Anchor Springs.
- 1.1.4 This Outline Construction Environmental Management Plan (CEMP) has been produced to support the planning application at the site with a view to managing and mitigating the construction-related impacts of the proposed development upon the neighbouring businesses, town centre as a whole and the wider highway network.
- 1.1.5 A CEMP provide a considered approach to how the potential impact of construction related traffic would be minimised and mitigated against.
- 1.1.6 As this is an outline document, the contents herein should be updated and expanded upon adoption as a full document for the proposed development, prior to the construction phase commencing. This will be the responsibility of the Main Contractor appointed for the works.
- 1.1.7 RGP has additionally prepared the following reports to support the planning application, and these should be read alongside the Construction and Environmental Management Plan:
- Transport Statement (ref. 2024/8391/TS01);
 - Travel Plan (ref. 2024/8391/TP01); and
 - Delivery and Servicing Management Plan (ref. 2024/8391/DSMP01).

2 SITE DETAILS

2.1 Site Location

2.1.1 The site is located centrally within Littlehampton, in close proximity of multiple amenities, services and public transport nodes, as illustrated in the figure below.

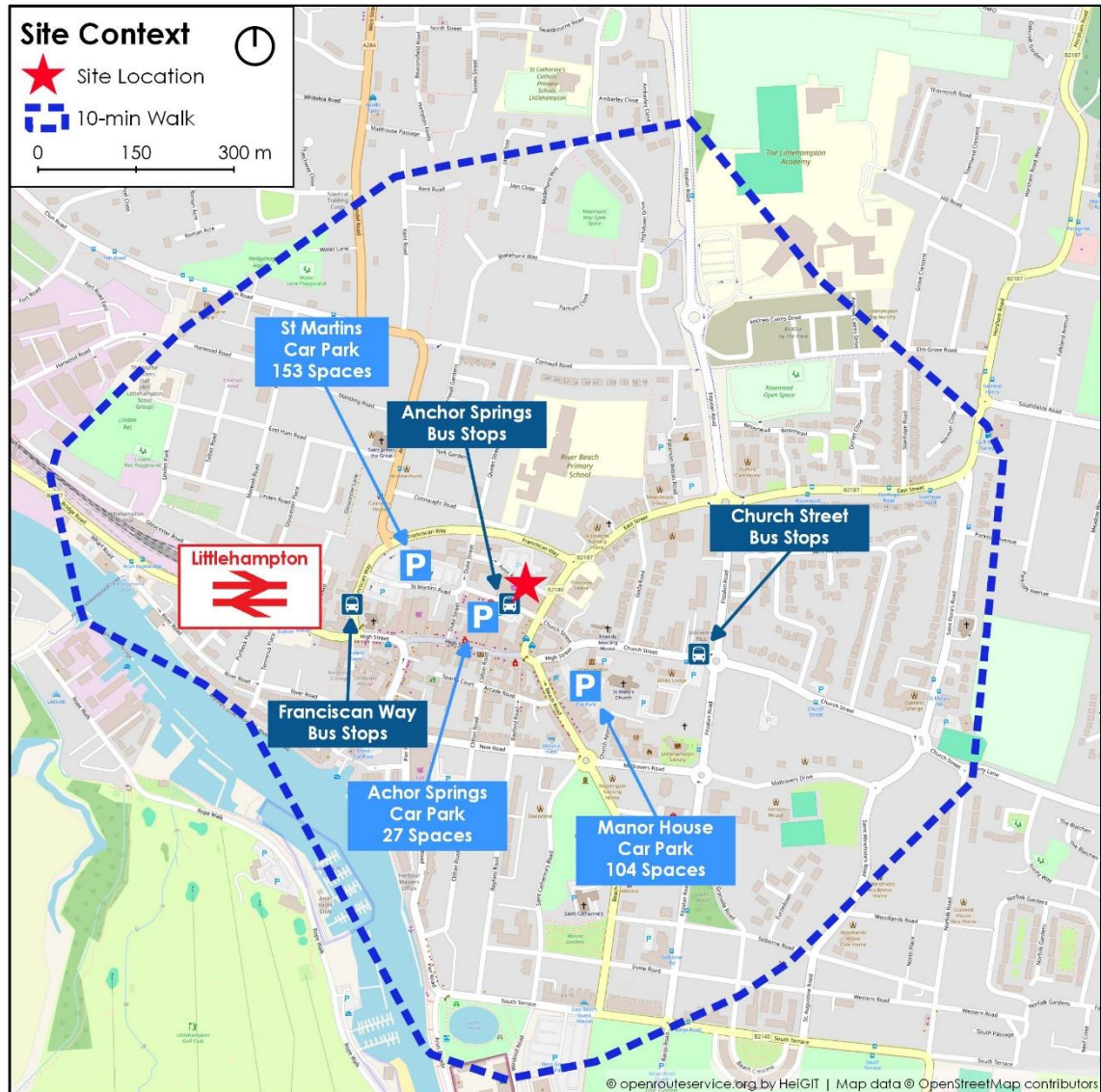


Figure 1 Site Location

2.1.2 The area surrounding the site is largely commercial in nature. As illustrated, there are a number of public car parks surrounding the site, including three within a short walking distance which offer a total of 284 spaces.

2.1.3 Vehicle access to the site is provided via the one-way Avon Road which runs from west to east between the main building and the hardstanding parking area to the rear. The photograph below shows Avon Road when looking east through the site.



Figure 2 Avon Road (February 2025)

2.1.4 As shown, a pedestrian crossing connects the on-site car park to the main building across the road, including tactile paving and dropped kerbs.

2.1.5 The photograph below shows the Avon Road and East Street Junction to the east of the site, permitting egress to the left and right only.



Figure 3 Avon Road / East Street Junction (February 2025)

2.1.6 As shown in the photograph, double-yellow line parking restrictions are present around the junction to maintain access and ensure that visibility is unobstructed.

- 2.1.7 Anchor Springs runs to the south of the site in a similar west-east arrangement. There are several bus stops located along the northern side of the road which flank the site, as shown in the photograph below.



Figure 4 Anchor Springs Bus Stops (February 2025)

- 2.1.8 The existing footway along Anchor Springs features bus shelters and waiting areas for passengers along the site frontage, with real-time information boards provided.

2.2 Highway Safety Appraisal

- 2.2.1 In order to assess the local highway safety record, collision data has been taken into consideration in line with current Department for Transport (DfT) guidance. Whilst it is the industry standard for the most recent five-year period to be assessed, an additional two years have been considered in light of the unrepresentative data from 2020 and 2021. The range for the analysis is therefore 2017-2023 with data sourced from Crashmap.co.uk.

- 2.2.2 The figure below illustrates the distribution of collisions within the immediate vicinity of the site.



Figure 5 Local Collision Distribution

2.2.3 There have been no recorded collisions at Avon Road, nor its junctions with Duke Street and East Street respectively.

2.3 Accessibility Credentials

Walking

2.3.1 As noted above, there are several local amenities and services within a short walk of the site across the town centre.

2.3.2 Notwithstanding this, the 'Planning for Walking' guidance (2015) produced by the Chartered Institution of Highways and Transportation (CIHT) has also been considered which provides information on the characteristics of pedestrian journeys, the benefits of walking and the legal framework that applies to pedestrians.

2.3.3 Further guidance set of by CIHT 'Providing for Journeys on Foot' (2000) sets out the considered industry standard in relating to distances that are desirable, acceptable and preferred maximum. This is summarised below on relative journey distances.

| Standard | Town Centre | Commuting / School | Elsewhere |
|-------------------|-------------|--------------------|-----------|
| Desirable | 200m | 500m | 400m |
| Acceptable | 400m | 1km | 800m |
| Preferred Maximum | 800m | 2km | 1.2km |

Figure 6 Desirable / Acceptable / Maximum Walking Distances (CIHT, 2000)

2.3.4 The figure below therefore illustrates an indicative 2km walking catchment from the site.



Figure 7 2km Walk Catchment

2.3.5 The application of the 2km walking catchment indicates that the site is well located for access to the town centre as well as peripheral areas to Littlehampton, from which construction personnel may reside.

Cycling

- 2.3.6 Over short distances, cycling is often quicker than using a car and more flexible than using public transport.
- 2.3.7 Cycling is an important part of the national and local transport policy agenda. An increased perception of cycling as a real alternative mode of transport to the car and growth in cycling as a leisure activity has increased demand for cycling.
- 2.3.8 Traditional DfT guidance outlines that many utility cycle trips are less than 3 miles (approximately 5km), but for commuter journeys a distance of over 5 miles (approximately 8km) is not uncommon. The CIHT's publication '*Cycle Friendly Infrastructure*' (1996), suggests that reasonable fit individuals can comfortably cycle a distance of 8km to workplace destinations.
- 2.3.9 In light of the above preferred maximum distances, the figure below illustrates an indicative 5km cycling catchment from the site.



Figure 8 5km Cycle Catchment

- 2.3.10 The analysis indicates that an extensive area can be accessed by bicycle, to include Crossbush, Angmering on sea and Ford.
- 2.3.11 National Cycle Network Route 2 is located towards the east of the site as shown within the figure below. This cycle route connects Littlehampton with Bognor Regis to the east, Route 2 overall connects St Austell to Dover with areas yet to be constructed, such as the route to the east towards Worthing.
- 2.3.12 The local topography across Littlehampton is generally flat and therefore considered conducive to journeys on foot and by bicycle.

Public Transport

- 2.3.13 Due to the central location of the site, construction personnel would be able to arrive at the hotel by public transport. As shown in **Figure 1** above, the site lies within close proximity of Littlehampton Railway Station and several local bus stops.

2.3.14 The Anchor Springs bus stops are located directly adjacent to the site along the primary hotel frontage. The table below provides a summary of the services that operate to / from the stops.

| Service | Route Summary | Frequency / Hours of Operation (Monday – Friday) |
|---------|------------------------------|--|
| 12 | East Preston - Littlehampton | Every hour / 10:20 – 15:22 |
| 500 | Westergate - Littlehampton | Every 30 minutes / 05:45 – 00:03 |
| 700 | Littlehampton - Brighton | Every 12 minutes / 05:40 – 22:34 |
| 9 | Arundel – Shoreham-by-Sea | Every hour / 07:24 – 17:45 |

Figure 9 Bus Service Summary

2.3.15 Local bus services would offer a viable means of travel for construction personnel.

2.3.16 The table below summarises rail services from Littlehampton Railway Station which is located circa 500m (7-minute walk) to the west of the site.

| Destinations | Frequency | Journey Time |
|-----------------|--------------------------|-------------------|
| Chichester | 1 train per hour | 20 minutes |
| Brighton | 1 train every 50 minutes | 50 minutes |
| London Victoria | Every 30 minutes | 1 hour 46 minutes |

Figure 10 Littlehampton Rail Service Summary

2.3.17 Rail travel therefore also represents a viable means for travel to / from the site.

2.4 Site Constraints

2.4.1 There are some notable constraints associated with the construction at the site. The following considerations and challenges are noted for this site:

- Bus stops along Anchor Springs;
- No substantial disruption is envisaged to the adjacent properties or businesses during the extent of the construction works, however, management measures would be put in place to ensure the potential impact would be minimised; and
- The site is close to the town centre and hence vehicle routing would be consideration of this and aim to avoid busier times / routes where possible.

- 2.4.2 The applicant is therefore committed to carrying out these works in the most practicably sustainable manner. Given the site's location adjacent to the wider highway network and that the adjacent properties would remain operational throughout the proposed construction process, the need to minimise the impact associated with the construction works is fully recognised and the proposed construction works have been considered on this basis within this CEMP.
- 2.4.3 The Main Contractor will be responsible for the monitoring of all construction works and traffic movements and ensuring the safety of the neighbouring uses, as well as passing vehicles, pedestrians and cyclists at all times.

3 PROGRAMME AND METHODOLOGY

[This section is to be updated further following the appointment of a Main Contractor].

3.1 Overview

3.1.1 The clearance and construction works are proposed to commence in *[date to be confirmed]*, lasting for approximately *[duration to be confirmed]*.

3.1.2 The schedule and construction activity has been assumed in light of the proposed scope of works as part of the scheme. This schedule will be further refined once a Main Contractor has been appointed:

- Site set-up and clearance;
- Superstructure;
- Cladding; and
- Building services, fit out and commissioning

3.1.3 The contact details of the chosen Main Contractor and management personnel, when appointed, shall be made available to all.

3.1.4 The contact details for the Main Contractor and Construction Manager (appointed under the Main Contractor) shall be clearly detailed at the front of the site for the duration of the development.

3.2 Construction Programme

[A full detailed programme of works shall be provided by the chosen Main Contractor prior to commencement of construction and monitored regularly throughout the process].

3.2.1 The table below summarises an indicative programme of works, highlighting the approximate duration of key phases of the project and the average number of construction vehicle movements during these phases.

| Phase | Duration | Construction Vehicles Per Day (Average) |
|--|----------|---|
| Site set-up and clearance | tbc | tbc |
| Superstructure | tbc | tbc |
| Cladding | tbc | tbc |
| Building services, fit-out and commissioning | tbc | tbc |

Figure 11 Construction Programme

3.2.2 All construction arrivals and departures would be managed closely by the Construction Manager and assisted by Traffic Marshals to prevent excessive impact on the local highway network.

3.2.3 The table above is indicative and would be updated with more accurate timings, once the Main Contractor has been appointed.

3.3 Proposed Working Hours

3.3.1 Construction works on the site will typically commence and finish at the following times:

- Monday to Friday: 07:00 – 18:00
- Saturday: 08:00 – 13:00; and
- No Sunday, Bank Holiday or Public Holiday working.

3.3.2 Under no circumstances will works outside of these hours be undertaken, unless otherwise agreed in advance with West Sussex County Council / Arun District Council.

3.3.3 Consideration will be given to any imposed vehicular restrictions within the town centre and disruption to the centre operations during these times would be limited where possible.

3.3.4 Furthermore, construction vehicle deliveries would be scheduled to occur outside of the peak hours on the highway network where feasible to minimise the residual impact of such activities.

4 SITE SETUP

[This section is to be updated further following the appointment of a Main Contractor and a full Construction Plan has been prepared]

4.1 Indicative Site Setup

4.1.1 The figure below provides an illustrative overview of the setup of the site, including means of access and potential hoarding lines. This should be considered for indicative purposes only and the locations as suggested may be subject to change and confirmation upon appointment of the Main Contractor.

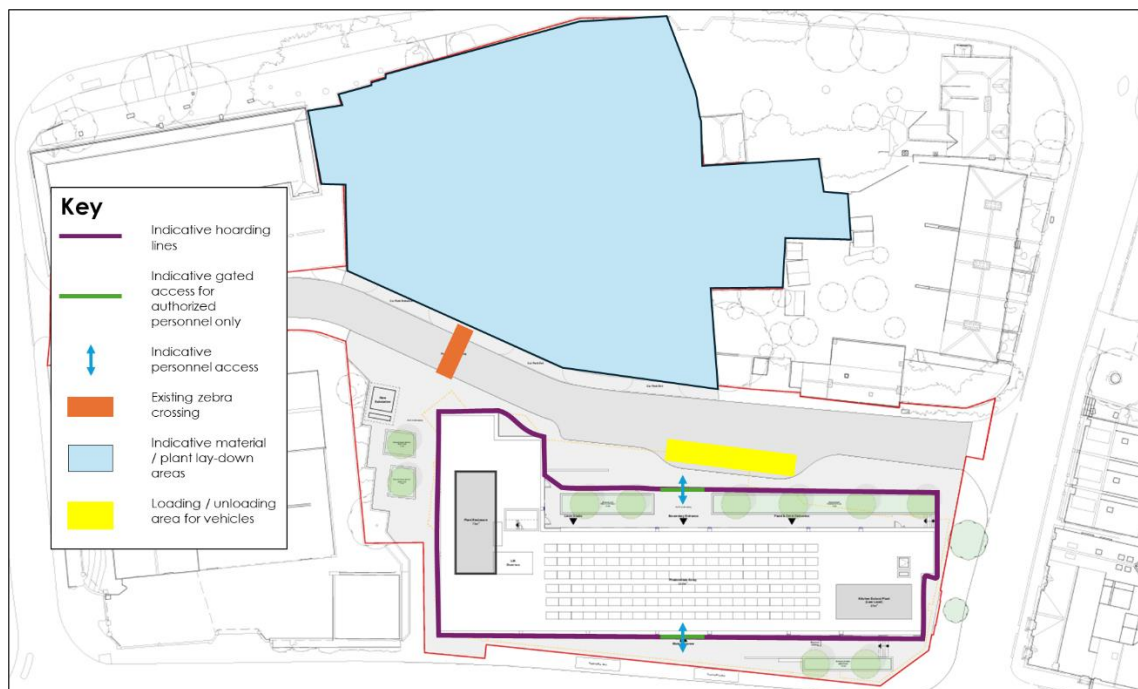


Figure 12 Indicative Site Setup

- 4.1.2 A more detailed plan will be prepared upon the appointment of a Main Contractor. Given the extents of the site and relatively at-grade alignment, materials and plant could be stored at several locations within the confines of the site itself, within the established hoarding lines.
- 4.1.3 Where possible, residual impacts on the adjacent roads would be minimised, including Anchor Springs along which the existing bus stops would remain operational. Where possible, the majority of activities would take place to the rear of the site via Avon Road and the on-site area of parking which could be utilised for material / plant laydown and limited contractor parking.
- 4.1.4 Prior to any works commencing it is proposed that a site hoarding is installed along the curtilage of the site works to prevent unauthorised access to the site and to warn of the potential dangers of construction zones. A site hoarding would ensure that safe access is maintained into the site for legitimate staff and construction deliveries only.

- 4.1.5 The extent of hoarding and any proposed temporary traffic management measures would be agreed with the Highway Authority in accordance with its licencing procedures. All necessary licences would be applied for by the Main Contractor in advance.
- 4.1.6 Appropriate signage will be installed at the site during the course of the works informing the public that no access to the site will be available. Appropriate hoarding will be provided around the site perimeter to protect the general public and shield the works from view as is practically possible.
- 4.1.7 Equipment and materials will be stored within the curtilage of the site to minimise disruption on the local highway network and neighbouring properties. All plant and material storage areas will allow for movement of personnel around the site and across Avon Road, where required.
- 4.1.8 Monitoring of the above elements will be undertaken by the Construction Manager, appointed by the Main Contractor, throughout the programme of works to ensure the safety of all those staff associated with the works and users of the public highway, at all times. The above elements will be amended, with additional mitigation processes put in place, as required, as the construction programme evolves.

4.2 Storage of Plan and Materials

- 4.2.1 All plant and materials would be stored within the site domain. Safe and adequate access will be provided to all parts of the site, and the site must be kept tidy. When the work has stopped for the day, the site will be secured, all ladders and access removed, the plant must be immobilised, and all hazardous materials will be safely stored.
- 4.2.2 Storage locations will need to be constantly reviewed as work progresses and the site conditions change. Signage and pedestrian protection will need to be constantly updated and communicated to all as these works develop.
- 4.2.3 There will be no storage of goods or waste on the local highway network with all plant and materials stored within the site domain at all times.

4.3 Contractor Parking

- 4.3.1 Site operatives and visitors will be encouraged to travel to / from the site via public transport. There are frequent bus and rail services available from within close proximity of the site. There would only be limited car parking accommodated within the site during construction, although the Anchor Springs or St Martins Lane car parks could provide vehicular parking if required by contractors.
- 4.3.2 The potential to provide lockers / storage areas on-site for tools and materials of construction staff would also be explored by the appointed Contractor to make sustainable travel more convenient for staff. This could be provided by way of a designated area within the storage compound.

5 VEHICLE ACCESS AND ROUTING

[This section is to be updated further following planning approval].

5.1 Types of Vehicles

5.1.1 It is anticipated that where possible, larger delivery vehicles would generally be used by the various trades employed and throughout the various phases of construction where possible in order to reduce the overall frequency of deliveries and minimise disruption on the local highway network.

5.1.2 The table below provides an indication of the types of vehicles anticipated during the construction process. The types of vehicles required for each phase of construction would be confirmed by the Main Contractor, however it is suggested that vehicle sizes would be restricted to enable safe manoeuvring around the site and local highway network as appropriate.

| Vehicle | Operation | Dimensions |
|-----------------------|--|--|
| Large Tipper Lorries | Transporting loose material to / from the site | Length: 10.2m Width: 2.5m Height: 2.9m |
| Medium Tipper Lorries | | Length: 8.2m Width: 3.5m Height: 2.9m |
| Small Tipper Lorries | | Length: 6.5m Width: 2.5m Height: 2.9m |
| Concrete Lorries | Mixing components and materials | Length: 8.4m Width: 2.4m Height: 4.0m |
| Flat-bed Trucks | Transport materials / Steels etc. | Length: 8.0m Width: 2.1m |
| Skip Lorries | Waste removal | Length: 6.3m Width: 2.9m Height: 2.9m |
| Transit Vans | Finishing materials and sanitary ware | Length: 5.3m Width: 2.0m Height: 2.5m |

Figure 13 Typical Construction Vehicles

5.2 Construction Vehicle Access

5.2.1 Drawing **2024/8391/006**, attached hereto, illustrates some of the typical construction vehicles accessing the site, namely a Medium Tipper and Concrete Mixer. As shown, these vehicles would be able to access and egress the proposed lay-by along Avon Road for loading / unloading activities.

5.2.2 All construction deliveries would be booked in advance with the Construction Manager and undertaken in a timely fashion to ensure only one delivery vehicle arrives at the site at any given time through the use of a delivery schedule and limited delivery hours. The number of construction deliveries each day would be restricted in order to minimise impact on the adjacent highway network.

5.2.3 All vehicle manoeuvres and loading / unloading would be assisted by a Traffic Marshal. Traffic Marshals would be situated at the site at all times when construction vehicles are expected to undertake deliveries to direct vehicles, pedestrians and cyclists in the locality.

5.3 Routing Strategy

5.3.1 The likely routing for construction traffic would be to / from the Strategic Road Network (SRN) in the form of the A27 to the north of the site. Due to the location of the site, busy roads around the town centre would be avoided, where possible, A-roads and B-roads would be used as the primary routes for traffic.

5.3.2 The figure below illustrates the potential construction traffic route from the A27 to the site.

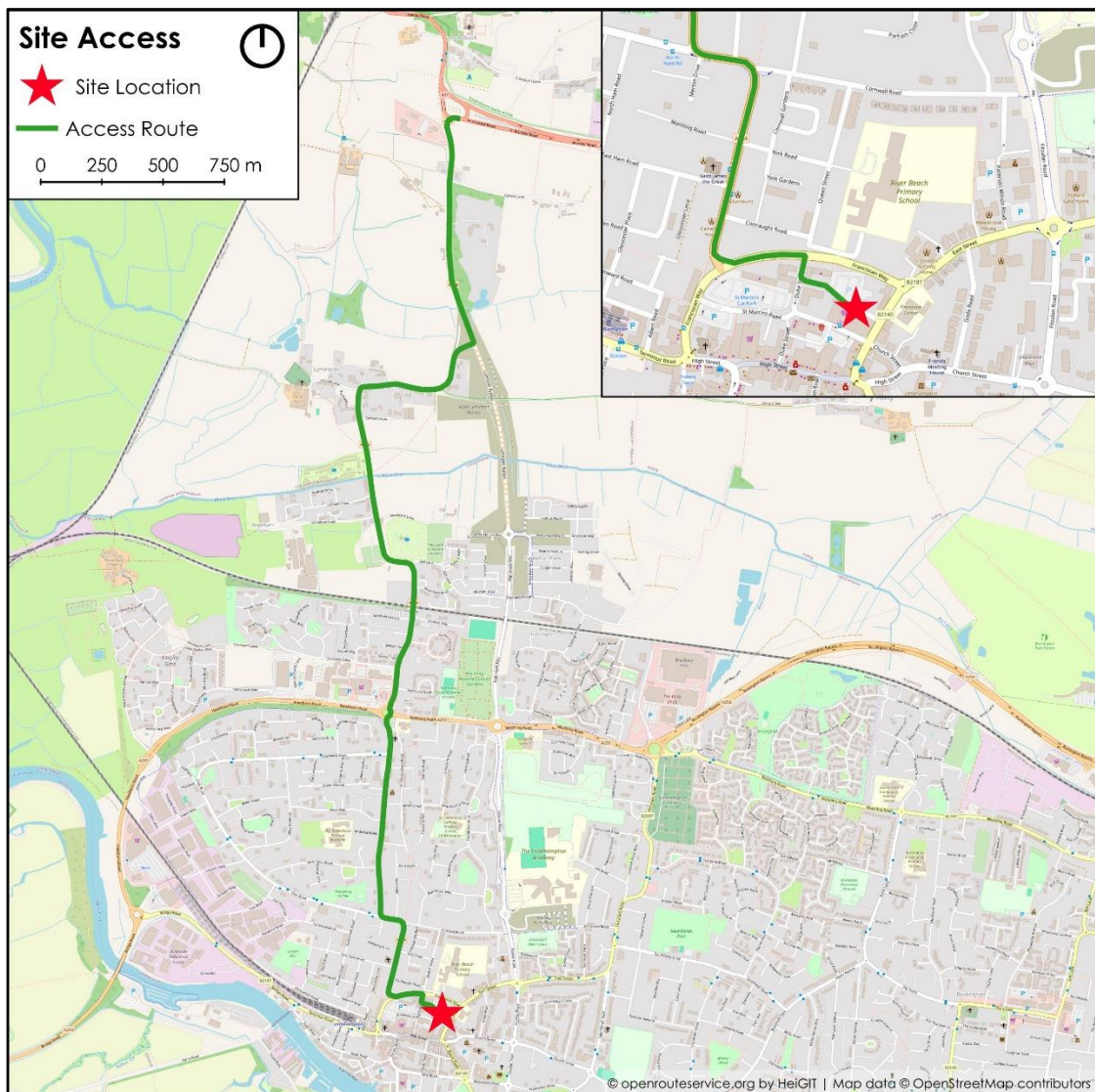


Figure 14 Indicative Construction Vehicle Access Route

5.3.3 As illustrated above, the potential route lies north of the site, the majority of the route utilises the A284 / Arundel Road.

5.3.4 The figure below shows the egress route for construction traffic from the site towards the A27.

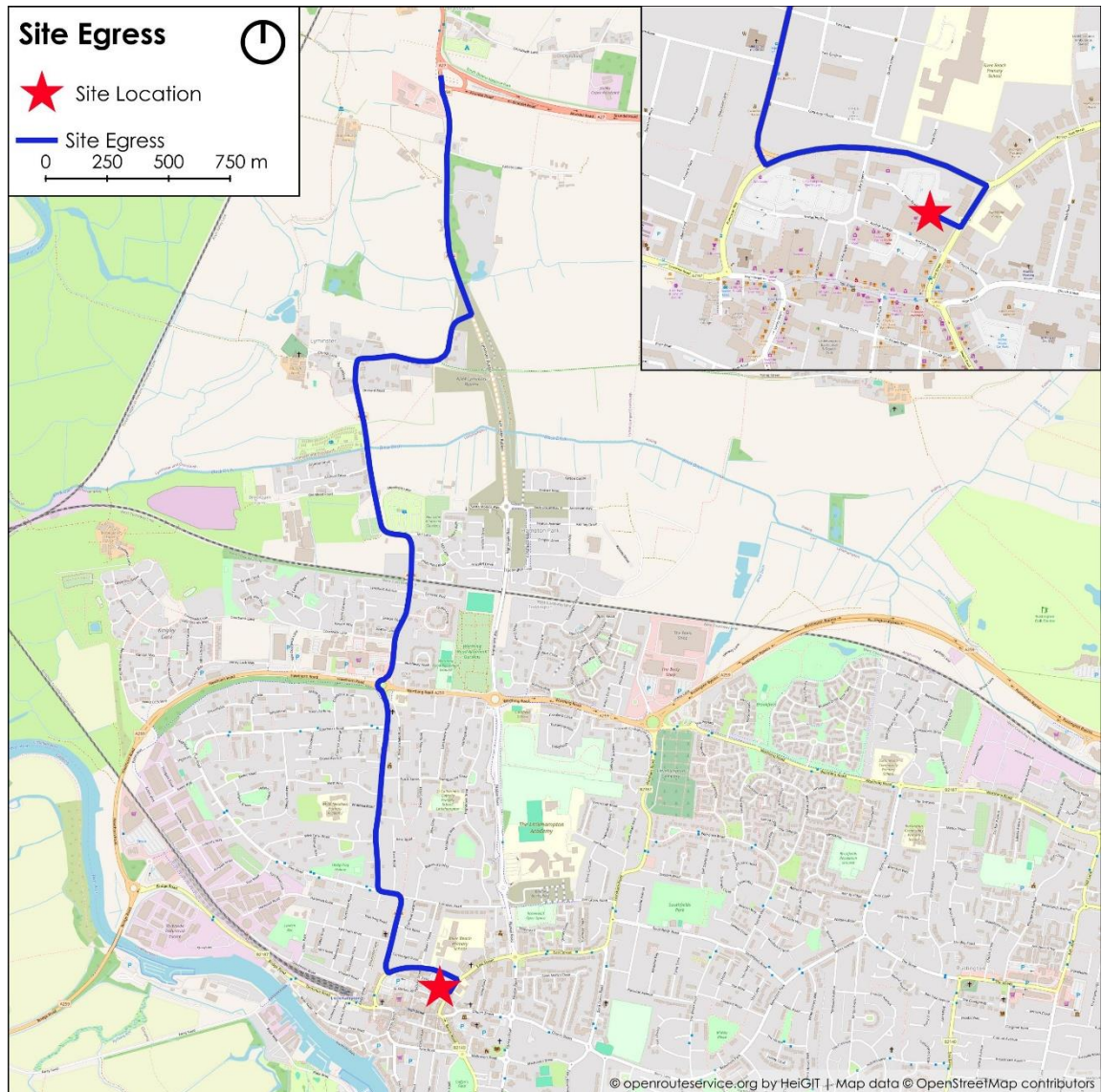


Figure 15 Indicative Construction Vehicle Egress Route

5.3.5 As shown, similar to the access the egress follows the A284 / Arundel Road. Additionally, the route also uses East Street for a brief part of the route.

5.3.6 The full routing strategy will be confirmed by the Main Contractor, once appointed.

5.3.7 Delivery drivers will be notified of the proposed access arrangements prior to their scheduled delivery time, in order to ensure vehicles adhere the agreed routes and access arrangements. Traffic Marshals shall be on hand at all times to assist vehicle manoeuvring as required.

5.4 Loading / Unloading and Storage Materials

- 5.4.1 Deliveries will be undertaken on a 'just in time' basis with all deliveries needing to be booked in 48 hours prior to the day of delivery. This will assist in the minimum amount of materials being stored within the site at any one time and improve delivery efficiency.
- 5.4.2 Any storage materials on-site will need to be constantly reviewed as work progresses and the site conditions change to ensure that all materials are accommodated on the site and not within the public highway.
- 5.4.3 Loading or unloading at any other time on weekdays or at other locations not stated within this document will in no instances be acceptable, unless otherwise agreed in advance.

6 STRATEGIES TO REDUCE IMPACT

6.1 Planned Measures Checklist

6.1.1 The appointed Construction Manager will ensure that all vehicles accessing and egressing the site adhere to the agreed strategies as set out within this Plan. In order to minimise congestion on local roads and inconvenience to third parties, the following principles are proposed:

- A designated loading area will be created at the site to enable the safe delivery of materials;
- Deliveries will only be permitted within the specified hours set out in this document to minimise the impact on pedestrians, cyclists and other road users;
- All deliveries will be booked in advance and managed by the Construction Manager, in liaison with the relevant supplier / construction company, in order to ensure that only one delivery vehicle arrives and / or departs the site at any given time;
- A delivery schedule will be prepared and kept up to date by the Construction Manager. The delivery schedule will detail the anticipated time of the delivery, contact details for the supplier, the type of delivery and the size of vehicle anticipated;
- All deliveries must be booked at least 48 hours in advance with the Construction Manager and made in accordance with the specified working hours outlined in this document;
- Any deliveries not booked may be turned away at the Contractor's expense;
- All deliveries will be supported by traffic marshals, as appropriate, to ensure the safe passage of materials to and from the site, without impacting on highway safety and other road users;
- Vehicles being off-loaded with goods at the site shall switch off their engines to avoid nuisance to the adjacent uses and to prevent dust generation;
- The contractor will sweep the roads and footways on the local highway network as required on a daily basis to remove any spoil or debris deposited on the highway resulting from the construction period if necessary. The appointed Construction Manager would ensure all construction vehicles depart the site in a clean and tidy manner and implement wheel washing facilities, if deemed required;
- Co-ordination will take place with other construction sites / businesses if found to be necessary when larger vehicles are required to deliver to the site;
- Any vehicle attempting to deliver outside these hours will be moved on and will only be permitted to return to site within the delivery hours as noted above;
- The Main Contractor will request all delivery vehicles to telephone ahead of arrival to site so that the necessary steps can be made to enable a smooth and efficient operation;
- Traffic Marshals will be informed and will be ready for arrival of the delivery, anticipating the type of delivery and unloading method to be utilised;

- All construction vehicles would adhere to the vehicle routing strategy detailed within this Plan;
- The operation of the construction site will comply with the Construction Logistics and Community Safety (CLOCS) Scheme, details of which are included at **Appendix B** of this Plan; and
- There would be no parking available on site for operatives or visitors, and these groups would therefore be encouraged to use alternative modes of travel where possible. All personnel will be briefed by the Main Contractor prior to their first visit to the site to make it clear that no parking is available at the site, however public town centre car parks are available at a cost to the individual. Designated lockers / storage areas will be provided at the site for tools and equipment in order to make sustainable travel on a daily basis more convenient for operatives and visitors.

6.1.2 A weekly review of forthcoming deliveries will be undertaken at a Logistics Progress meeting and the deliveries for the coming week will be agreed with the Construction Manager in advance.

6.2 Measures to improve Sustainability

6.2.1 In order to encourage the use of sustainable travel and reduce reliance upon private car use by operatives and visitors, a number of travel planning measures will be considered by the Main Contractor. The following principles will be followed:

- Use of local suppliers, as far as reasonably possible, to reduce distance travelled and associated vehicle emissions;
- Use of local labour / operatives who are more likely to reside within the local area and therefore travel by sustainable modes;
- Providing operatives within timetable bus / rail information, if requested;
- The potential to provide lockers on-site for tools and materials of construction staff will be explored by the Main Contractor to make sustainable travel more convenient; and
- An induction programme for all staff, making them aware of the zero-parking available and convenient access via sustainable modes. Car sharing will also be encouraged as a suitable alternative.

6.3 Traffic Marshals / Banksman

6.3.1 Where necessary, a suitably qualified Traffic Marshal / Banksman would be appropriately located at the site's frontage and on the site during scheduled deliveries, to direct both vehicles and pedestrians in the locality.

6.3.2 The provision of a qualified Traffic Marshal / Banksman will ensure that all vehicle and pedestrian movements into / out of the site are controlled and managed appropriately. It is proposed that all activity involving vehicles turning around or reversing on the public highway will be assisted.

6.4 Staff Travel Plan

6.4.1 Limited car parking provisions will be provided on-site for staff, given the constraints within the vicinity of the site and the extensive availability of public transport services within the local area. As noted, construction personnel would be free to use the local public car parking offerings at their discretion.

6.4.2 As discussed in **Section 2** of this report, there are frequent public transport services available from within close proximity of the site. In order to encourage the use of sustainable travel and reduce reliance upon private car use by staff, a number of travel planning measures will be considered by the Main Contractor. The following principles will be followed:

- Use of local suppliers, as far as reasonably possible, to reduce distance travelled and associated vehicle emissions;
- Use of local labour / operatives who are more likely to reside within the local area and therefore travel by sustainable modes;
- Providing operatives within timetable bus / rail information, if requested;
- The potential to provide lockers on-site for tools and materials of construction staff will be explored by the Main Contractor to make sustainable travel more convenient; and
- An induction programme for all staff, making them aware of the zero-parking available and convenient access via sustainable modes. Car sharing will also be encouraged as a suitable alternative.

7 ENVIRONMENTAL STRATEGY

7.1 Noise

7.1.1 Construction works are generally highly noise generating sources of activity and given that the adjacent uses would be operational throughout the construction process, a number of mitigation measures will be enforced and / or considered to suppress noise and vibration generated on the site.

7.1.2 The Construction Manager will be responsible for the monitoring and management of noise at the site and adhering to the Noise Working Standards as set out by the Local Authority Environmental Health Department.

7.1.3 If the measures noise level rises significantly above the predicted noise level, or in the event that a noise complaint is received locally, the Construction Manager will investigate the cause and the noise levels will be reduced, if it is reasonably practicable to do so.

7.1.4 A number of mitigation measures will be considered to suppress noise generated on the site, including:

- Ensuring that all work is undertaken within the restricted working hours;
- Using 'silenced' plant and / or equipment and low vibration construction methods, wherever possible;
- Using mains power instead of generators, wherever possible;
- Ensuring all operatives are professionally trained and provided with ear and eye protection;
- Ensuring delivery drivers turn off their engines upon arrival and when loading / unloading goods;
- Using protection plates and mobile screens around those parts of the site likely to generate significant levels of noise. Such screens will have sufficient mass as to be able to resist the passage of the sound;
- Strategically placing noise generating plant as far as possible from sensitive receptors and the general public; and
- Ensuring all deliveries are scheduled and assisted by a Traffic Marshal to ensure deliveries do not need to wait to park. Idling will in no instances be acceptable.

7.1.5 The above list of mitigation measures is not exhaustive, and the Construction Manager (once appointed) is encouraged to investigate other potential measures throughout the construction process.

7.2 Vibration Levels

- 7.2.1 In the case of vibration, measured vibration levels shall be compared with the criteria in BS 5228:2009 part 2 (i.e. 1mms¹ - PPV for potential disturbance in residential properties and using a suggested trigger criterion of 2mms¹ - for commercial properties).
- 7.2.2 Lower limits must be agreed with the Council if there is a risk that vibration levels may interfere with vibration sensitive equipment or other vibration sensitive objects.

7.3 Potential Impacts on Air Quality

7.3.1 Impacts on air quality can arise as a result of construction activities, particularly the annoyance of dirt, dust and debris. A number of mitigation measures which are to be enforced at the site are detailed below:

- Using water spray to reduce dust generation;
- Using protection plates and mobile screens;
- A wheel washing facility would be implemented on the site, if necessary. The appointed Construction Manager would ensure all construction vehicles depart the site in a clean and tidy manner. The Construction Manager would also investigate the potential to provide a more suitable location for the facility, should it be deemed necessary, as works progress;
- Materials / waste stored on the site should be covered, particularly outside of working hours. The storage of materials or waste on the public highway and at other locations will in no instances be acceptable;
- All vehicles carrying materials to / from the site should be covered to reduce the likelihood of spillages or leaks;
- Ensuring the road edges around the site and within the site access are swept by hand by staff at the end of the day;
- All construction vehicles will follow the designated routes to reduce the impact of vehicle emissions; and
- Special provisions to be provided and agreed with the Highway Authority for any materials containing asbestos, as appropriate.

7.4 Waste Management

7.4.1 The Main Contractor (when appointed) will be responsible for the careful management of waste as a result of construction works at the site. This will be achieved by adopting the key principle of the Waste Hierarchy, as outlined by the Department for Environment Food and Rural Affairs (DEFRA) and illustrated in the figure below.

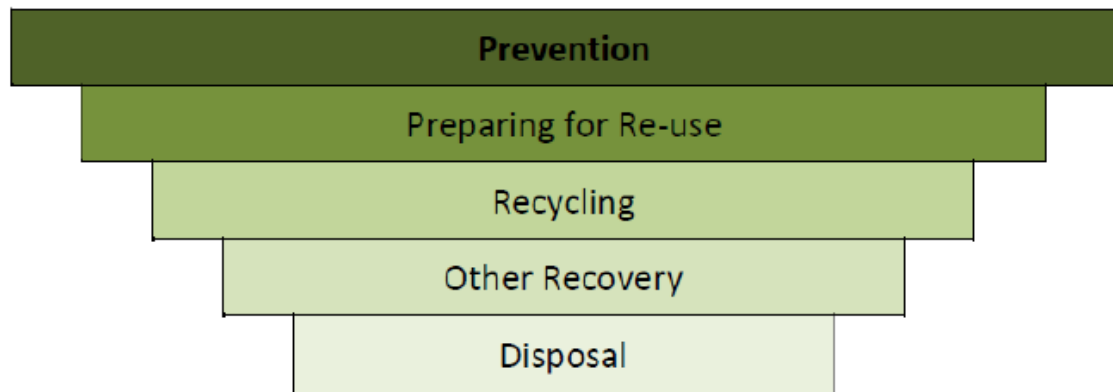


Figure 16 DEFRA Waste Hierarchy (Preferred to Least Preferred Option)

7.4.2 This gives top priority to preventing waste in the first instance and provides a procedure to follow when waste is created, including re-using, recycling, recovery and the disposing of waste as a worst case.

Prevention

7.4.3 The primary aim with regards to the management of waste during the construction process is prevention, wherever possible, by way of utilising materials which are more durable, and which are less hazardous to the environment, staff and the local community.

7.4.4 Once the Main Contractor is appointed it is anticipated that the site will operate on a 'just-in-time' basis for all goods and waste, to ensure the minimum amount of goods and waste are stored on the site at any given time.

Re-use and Recycling

7.4.5 Opportunities for on-site re-use and recycling of materials will be sought wherever practicable. It is acknowledged that there will be limitations on re-use of existing materials already on the site given that the subject site currently comprises a car park with little in the way of physical structures.

7.4.6 Prior to commencement, a pre-clearance / demolition audit will be carried out which will consider the potential for recovering as much material as possible. This will be summarised into a bill of quantities, setting out the anticipated quantities of surplus materials, for example.

7.4.7 Where it is not practical to re-use existing materials on the site, recycling will be sought as a suitable alternative ahead of the possibility of disposal. This could include, but is not limited to, turning waste into a new substance or material such as composting, for example.

Other Recovery

- 7.4.8 Other opportunities for recovering products and goods will also be considered in the event that recycling cannot be utilised. This may include using different forms of energy recovery technologies such as combustion with energy or anaerobic digestion, for example.

Disposal

- 7.4.9 The disposal of waste will by no means be encouraged and will only be sought where disposal is the only option.
- 7.4.10 At no time will the dumping of waste be permitted both on the site or off the site. Any waste must be collected from the site and disposed of by a registered licensed contractor at a licensed landfill site suitable for the type of waste generated.
- 7.4.11 Burning of surplus material or material arising from the site will not be permitted within the site.

8 IMPLEMENTING, MONITORING AND UPDATING

8.1 Overview

8.1.1 The Works Manager (when appointed) will own and manage the implementation of this Plan. The following highlights the key roles for this position:

- Ensure a site-specific risk assessment is carried out for all traffic activities (arrival, departure, (un)loading, movement, maintenance);
- Ensure a project induction is available to all staff and drivers, which shows key routes, restrictions etc.;
- Monitoring and reviewing health and safety performance of all parties;
- Ensure there are adequate emergency procedures in place for all foreseeable events i.e. traffic issues, spills, medical evacuation, fire, and traffic management control;
- Provision of materials / equipment to support the strategy;
- Provision of competent resources;
- Facilitate deliveries and management of delivery / logistics strategy; and
- Ongoing review and updating of this Plan as the project develops and site conditions change.

8.2 Communication

8.2.1 The contract details of the Works Manager including an emergency out-of-hours contact will be published at the front of the site.

8.2.2 Contract information for a number of key external groups has also been included within the table below.

| Name | Address | Contact |
|-------------------------|---|---------------|
| HSE | Health and Safety Executive, Woodlands, Manton Lane, Manton Lane Industrial Estate, Bedford, MK41 7LW | 0300 123 4500 |
| Environment Agency | Environment Agency Head Office, Horizon House, Bristol, BS1 5AH | 0370 850 6506 |
| Local Highway Authority | West Sussex County Council, County Planning, Chichester, PO19 1RH | 01243 642 118 |

Figure 17 External Key Contact Details

8.3 Reporting and Review

8.3.1 The Works Manager would liaise with any sub-contractors and suppliers on a regular basis with respect to incoming deliveries.

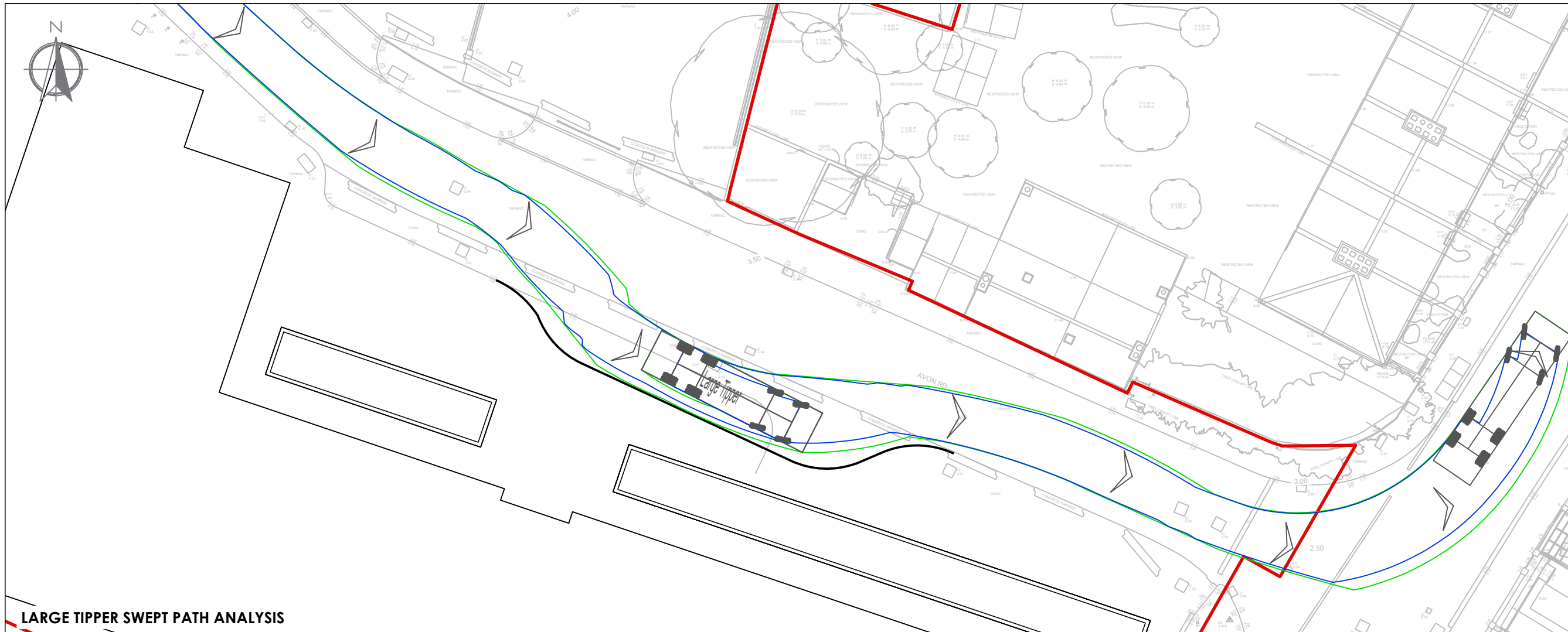
- 8.3.2 Any outstanding details within this outline CEMP would be confirmed within a full CEMP in any event. The Works Manager will review the full CEMP regularly as conditions change. Records of any updates / revisions will be maintained by the Works Manager.
- 8.3.3 All records will be held on file, on site, including all certificates and inspection records for all plant, equipment and lifting etc. that are required for traffic management and works purposes.

8.4 Breaches and Complaints

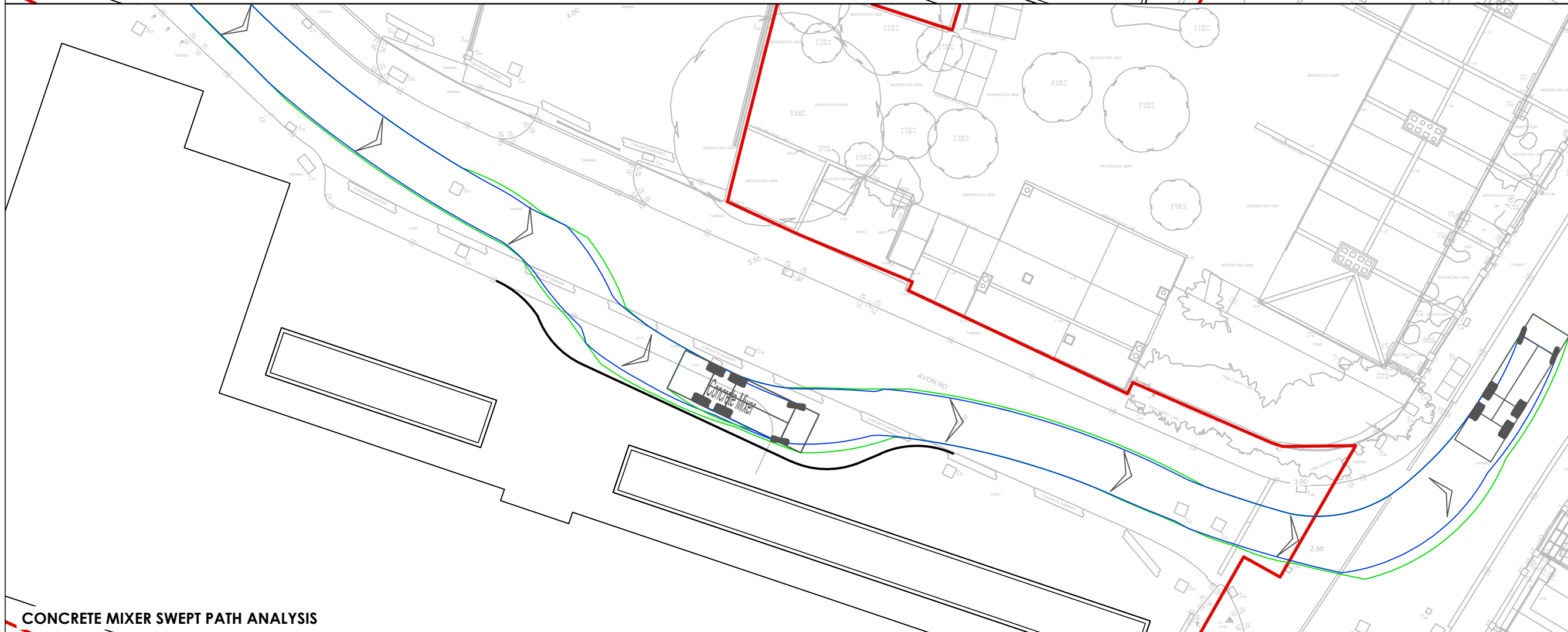
- 8.4.1 The contact details of the Construction Manager including an emergency out of hours contact will be published at the front of the site and will seek to respond to any formal complaint received within seven business days with respect to community concerns, vehicle routing issues and unacceptable parking by staff, for example.
- 8.4.2 The Construction Manager will be expected to develop a constructive relationship with those in the immediate vicinity and community of the development. A forum for consultation with the public will be set up, where feedback will be encouraged and updates on the development will be posted to keep the community up to date with activities on site. A letterbox drop to inform local personnel of construction timing, work duration and what works are occurring at what times will also be considered.



DRAWINGS



LARGE TIPPER SWEEP PATH ANALYSIS

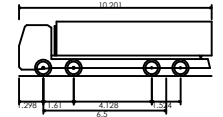


CONCRETE MIXER SWEEP PATH ANALYSIS

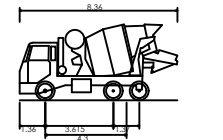
NOTES

This drawing has been prepared for the purpose of planning discussions and does not constitute a detailed design drawing, or construction drawing. A Design Hazard Inventory has been prepared by RGP setting out the hazards which have been designed out. This is available upon request.

— SITE BOUNDARY



Large Tipper
 Overall Length 10.20m
 Overall Width 2.49m
 Overall Body Height 2.89m
 Min Body Ground Clearance 0.34m
 Track Width 2.47m
 Lock to lock time 6.00m
 Kerb to Kerb Turning Radius 11.550m



Concrete Mixer
 Overall Length 8.36m
 Overall Width 2.39m
 Overall Body Height 4.02m
 Min Body Ground Clearance 0.358m
 Max Track Width 2.413m
 Lock to lock time 6.90m
 Kerb to Kerb Turning Radius 8.210m

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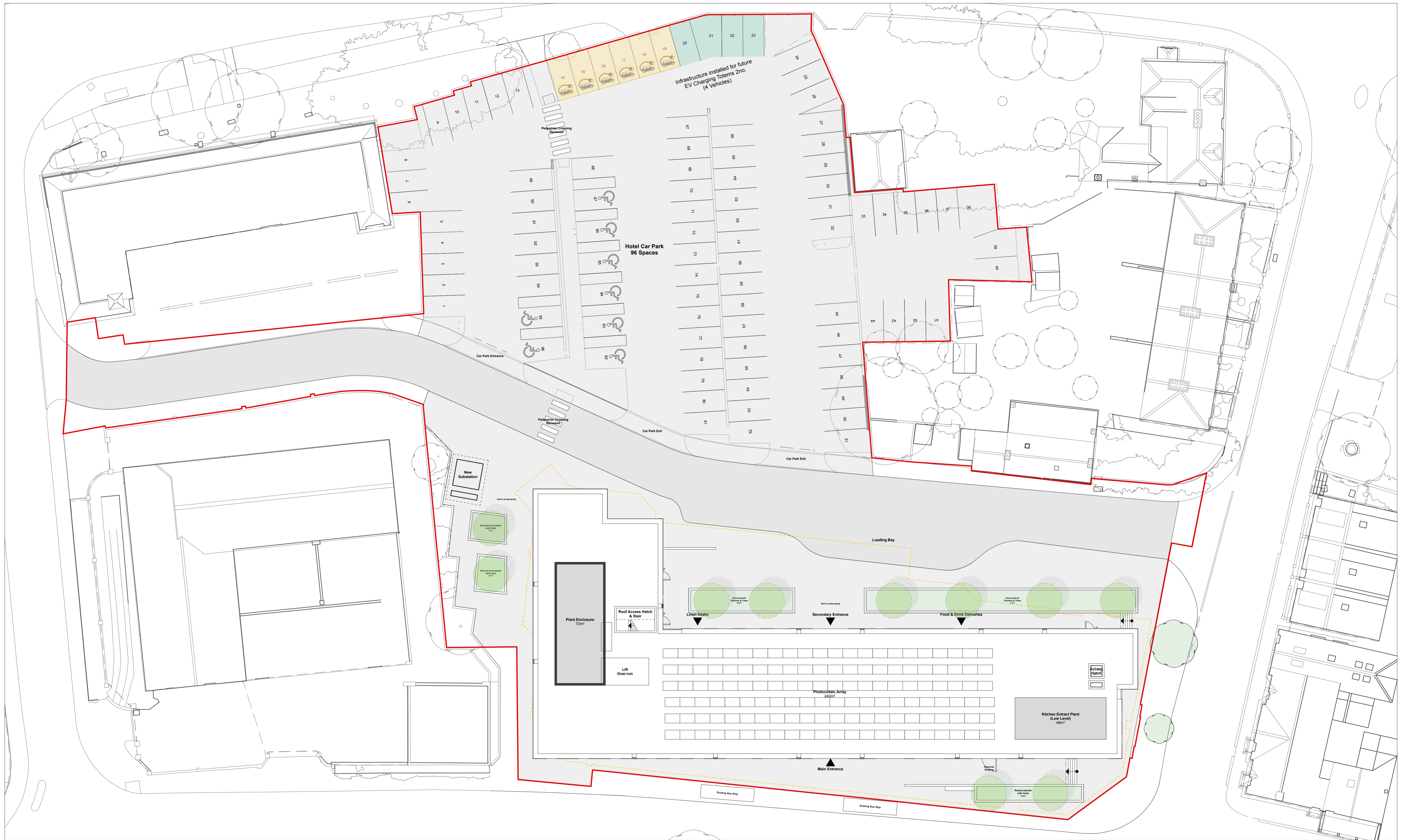
| Rev. | Drawn | Comments | Date |
|------|-------|------------------------------|----------|
| P2 | GE | UPDATED LAYOUT & LOADING BAY | 14/04/25 |
| P1 | GE | FIRST ISSUE | 08/04/25 |



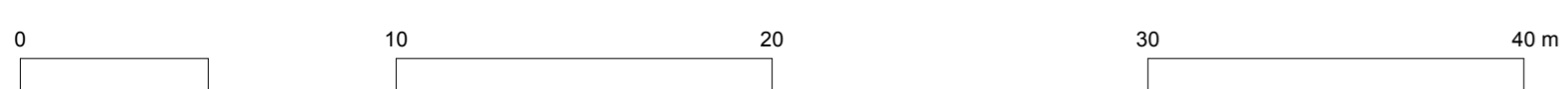
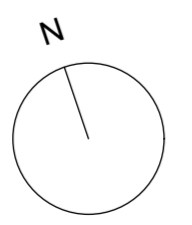
| | | | |
|---------------|---|------------|----|
| Client | Premier Inn Hotels | | |
| Project | Premier Inn Littlehampton | | |
| Drawing Title | Swept Path Analysis Construction Vehicle | | |
| Drawing No. | 2024/8391/006 | Rev. | P2 |
| Scale | 1:250 | Drawn By | GE |
| | | Checked By | JC |
| | | | A3 |



APPENDIX A

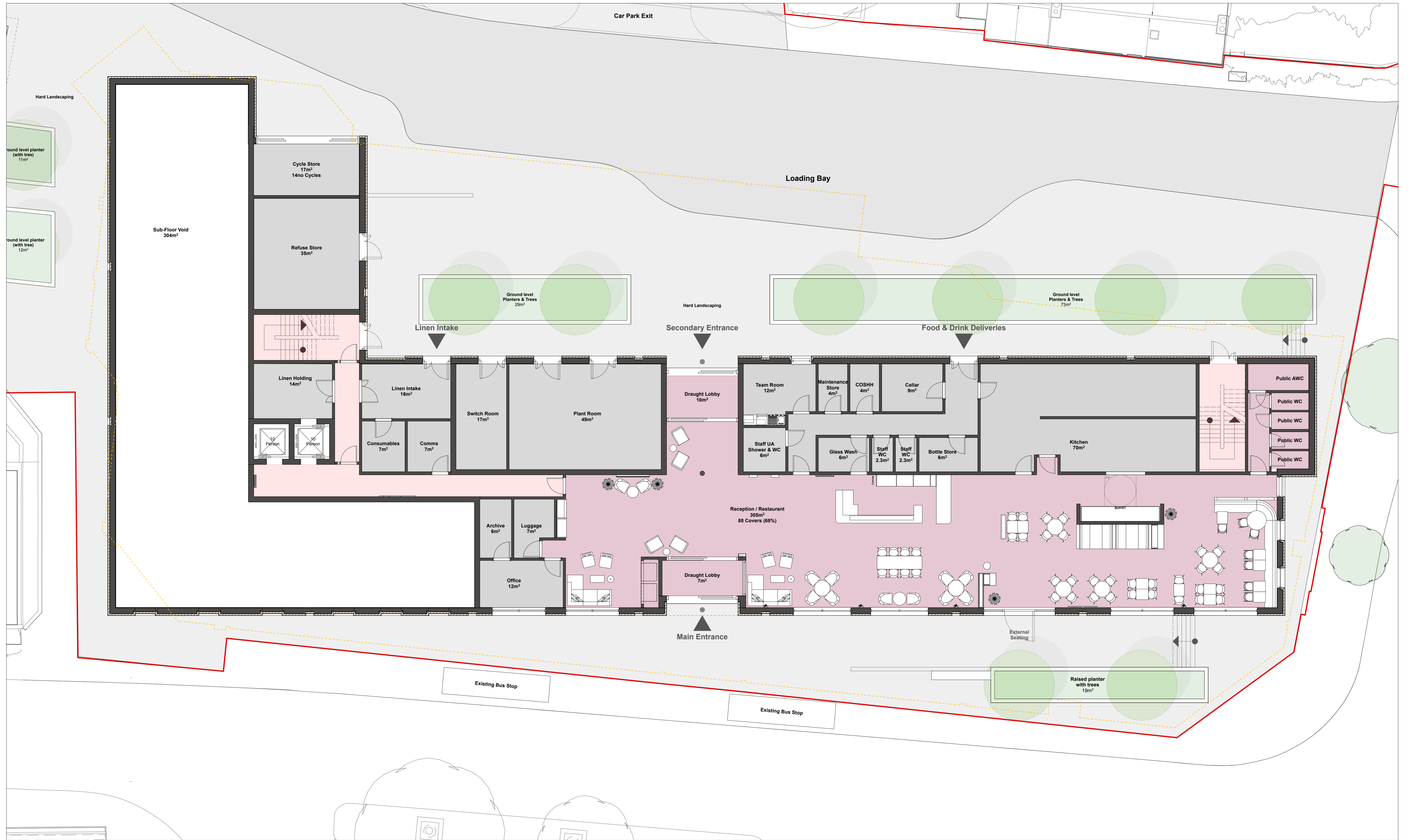


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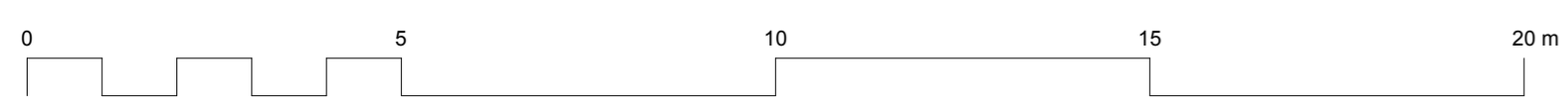
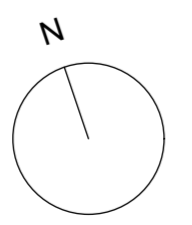


| | | | | | | |
|--|------------------------|---|----------------------------|----------------------------|----------------------|---------------------------|
| Client Premier Inn Hotels | | 1 Brooklands Yard Southover High Street Lewes East Sussex BN7 1HU 01273 479434 www.axiomarchitects.co.uk | | AXIOM ARCHITECTS | | |
| Project Anchor Springs Littlehampton Premier Inn | | Drawing Proposed Site Plan | | | | |
| Drawing No. 6122-P- | Revision 002 | Date 08/04/25 | Scale 1:200 @ A1 | Drawn LB | Checked LB | Status PLANNING |

| Rev | Date | Description | By | Chk |
|-----|------|-------------|----|-----|
| | | | | |



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| <p>Client Premier Inn Hotels</p> <p>Project Anchor Springs Littlehampton Premier Inn</p> <p>Drawing No. 6122-P-</p> | <p>1 Brooklands Yard Southover High Street Lewes East Sussex BN7 1HU 01273 479434 www.axiomarchitects.co.uk</p> <p>ARCHITECTS</p> <p>Drawing Proposed Ground Floor Plan</p> | <p>AXIOM ARCHITECTS</p> | | | | | | | | | | | | | | | | | | | | | | | | |
|---|---|------------------------------------|-------------|-------|---------|----------|--|--|--|--|---|-------------|----------|------|-------|-------|---------|--------|---------|-----|----------|------------|----|----|----------|--|
| <table border="0"> <thead> <tr> <th>Rev</th> <th>Date</th> <th>Description</th> <th>By</th> <th>Chk</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> | Rev | Date | Description | By | Chk | | | | | | <table border="0"> <thead> <tr> <th>Drawing No.</th> <th>Revision</th> <th>Date</th> <th>Scale</th> <th>Drawn</th> <th>Checked</th> <th>Status</th> </tr> </thead> <tbody> <tr> <td>6122-P-</td> <td>100</td> <td>08/04/25</td> <td>1:100 @ A1</td> <td>LB</td> <td>LB</td> <td>PLANNING</td> </tr> </tbody> </table> | Drawing No. | Revision | Date | Scale | Drawn | Checked | Status | 6122-P- | 100 | 08/04/25 | 1:100 @ A1 | LB | LB | PLANNING | |
| Rev | Date | Description | By | Chk | | | | | | | | | | | | | | | | | | | | | | |
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| Drawing No. | Revision | Date | Scale | Drawn | Checked | Status | | | | | | | | | | | | | | | | | | | | |
| 6122-P- | 100 | 08/04/25 | 1:100 @ A1 | LB | LB | PLANNING | | | | | | | | | | | | | | | | | | | | |



APPENDIX B

APPENDIX: CLOCS Standard for construction logistics: Managing work related road risk (WRRR)

| CLOCS Requirement | Further Information |
|---|--|
| Operations | |
| <p>Quality operation – current certification with the Fleet Operator Recognition Scheme (FORS) Bronze accreditation (or equivalent)</p> | <p>www.fors-online.org.uk</p> |
| <p>Collision reporting - Conduct collision reporting, investigation and analysis</p> | <p>Included as part of FORS: CLOCS Manager is a free collision reporting tool available to all operators: www.clocs-manager.org.uk</p> <p>Free collision reporting toolkit available: www.clocs.org.uk/clocs-guides/</p> |
| <p>Traffic routing – adhere to any client specified routes</p> | <p>Follow client instruction</p> |
| Vehicles | |
| <p>The following vehicle safety equipment shall be fitted to vehicles over 3.5 tonnes:</p> | <p>CLOCS Guide: Vehicle safety equipment www.clocs.org.uk/clocs-guides/</p> |
| <p>Prominent signage warning other road users not to get too close to the vehicle</p> <p>Side-guards on both sides of exempt vehicles</p> <p>Class V and VI 'close proximity' mirrors to exempt vehicles</p> <p>Close proximity warning system and/or camera system and/or vision-aid fitted to HGVs</p> <p>Left turn audible vehicle manoeuvring warnings</p> | <p>Further information and discounts on stickers and other equipment available through FORS: www.fors-online.org.uk/cms/contractors/fors-offers/ www.fors-online.org.uk/cms/discount-equipment/</p> <p>Other services: www.fors-online.org.uk/cms/discount-services/</p> |
| Drivers | |
| <p>Approved driver training in vulnerable road user safety</p> | <p>SUD courses and other approved courses detailed on FORS website: www.fors-online.org.uk/cms/training-discounts/</p> |
| <p>Driver licences checked through DVLA</p> | <p>Free through DVLA online check</p> <p>Discounts on DVLA license checking services available through FORS: www.fors-online.org.uk/cms/discount-services/</p> |





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