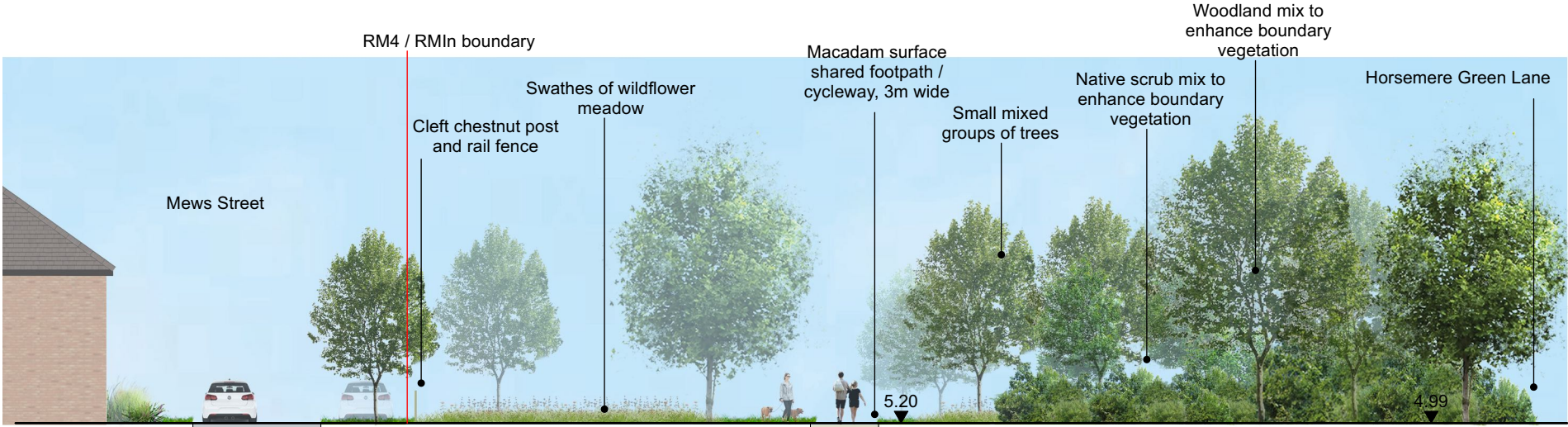
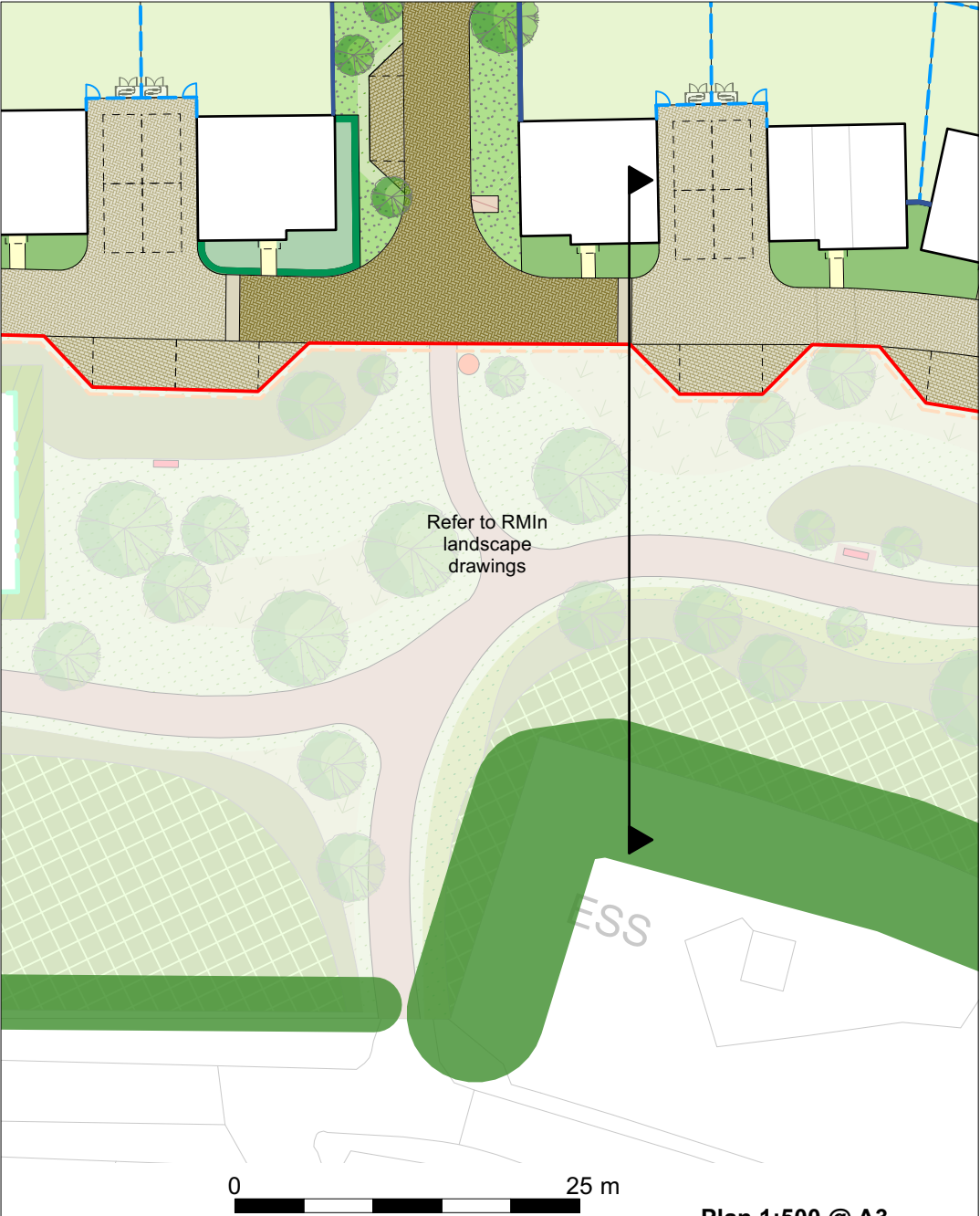
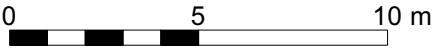




Section 1:200 @ A3



Plan 1:500 @ A3

- RM4 Boundary
- Cleft chestnut post and 2 rails fence 1.2m high
- Mews Street: Charcon Woburn Original in Brindle, 200x134mm, 45deg herringbone  
Kerb: Charcon Eco Countryside Kerb 145x255mm, 50mm upstand
- Semi Private Drives: Charcon Woburn Original in Rustic, 200x134mm, 45deg herringbone  
Kerb: Charcon Eco Countryside Classic Flat Top Edging 50x250mm, 50mm upstand
- Parallel parking bays: Charcon Woburn Original in Brindle, 200x134mm, stretcher bond  
Kerb: Charcon Eco Countryside Kerb 145x255mm, 50mm upstand
- Parallel parking bays: Charcon Woburn Original in Rustic, 200x134mm, stretcher bond  
Kerb: Charcon Eco Countryside Kerb 145x255mm, 50mm upstand
- Driveways: Charcon Woburn Rumbled in Autumn, 200x134mm, stretcher bond
- Rumble Strips: Charcon Woburn Original in Graphite, 200x134mm, stretcher bond
- Private footpaths: Charcon Stonemaster Flag, 450x450mm in sandy buff, stack bond
- Macadam surface footpath/cycleway, 3m wide  
Kerb: Charcon British Standard Flat Top Edging 50x250mm, laid flush
- Bin Collection Points: Charcon Woburn Rumbled in Autumn, 134x134mm, stack bond
- Proposed bench
- Waste bin
- Existing vegetation to be retained  
(Refer to arboriculturists AIA)
- Proposed trees
- Low single-species hedgerow
- Native scrub
- Woodland structure planting
- Amenity grass
- Long grass margins
- Wildflower meadow
- Private ornamental planting
- Rear gardens
- Private front lawn

Ford Airfield  
Vistry

|                               |             |             |
|-------------------------------|-------------|-------------|
| Climping Edge Transect<br>RM4 |             |             |
| Status:                       | Drawn by:   | Checked by: |
| For Planning                  | DL          | TF          |
| Project Number:               | Scale @ A3: | Date:       |
| 180641                        | 1:500       | 08/2024     |
| Drawing Number:               | Revision:   |             |
| TOR-RM4-XX-DR-L-P-029         | B           |             |

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