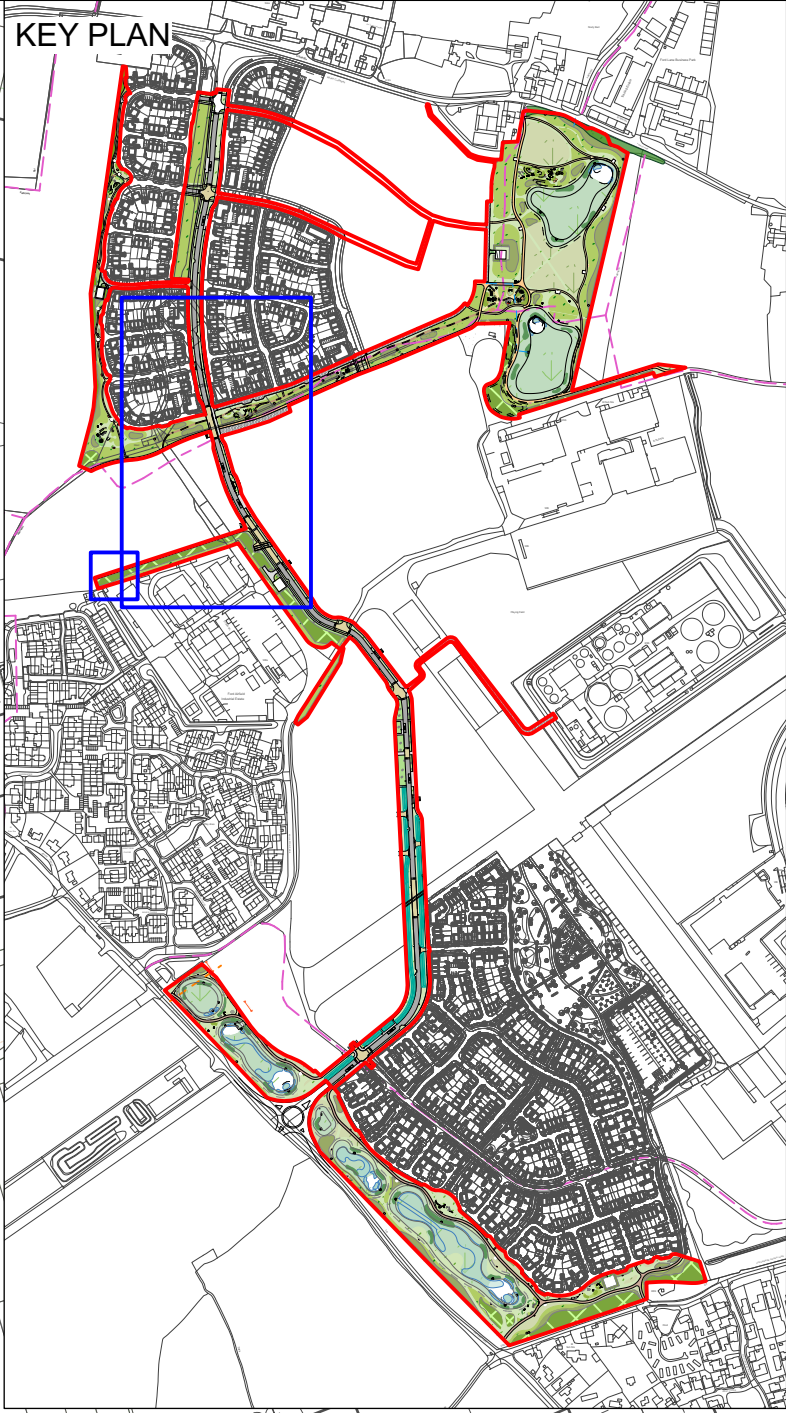




- Infrastructure reserved matters application boundary
- Proposed trees and predicted crown growth at maturity (at 25 years based on Hillier guidance)
- Existing vegetation to be retained (Refer to arboriculturists AIA)
- Vegetation to be removed (Refer to arboriculturists AIA)
- Indicative street light and 5m horizontal clearance
- Carriageway - Asphalt in accordance with engineers drawings
- Footway / 3.5m shared cycleway - Asphalt in accordance with engineers drawings
- Raised table and ramps - Block paving in accordance with engineers drawings
- Block paving in accordance with engineers drawings
- Block paving in accordance with engineers drawings
- Long grass
- Short mown grass
- Wildflower meadow (SuDs features)
- Flowering lawn
- Woodland structure planting
- 4m high acoustic fence
- Public art
- Steel wire mesh security fence in green 1.8m high

Refer to drawing RMIN-XX-DR-L-P-010 for associated plant schedules

Notes/Revisions		
B	12/08/2024	Minor amendments following client comments
C	17/12/2024	Post submission amendments in response to consultee comments
D	09/01/2025	Post submission amendments in response to consultee comments

Ford Airfield
Vistry

Reserved Matters Infrastructure
Primary Street North 2 of 2

Status:	For Planning	Drawn by:	GP	Checked by:	TF
Project Number:	180641	Scale @ A1:	1:500	Date:	06/2024
Drawing Number:	TOR-RMIN-XX-DR-L-P-005	Revision:	D		

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